

4-3-040 COMMERCIAL CORRIDOR BUSINESS DESIGNATIONS:

A. PURPOSE:

These regulations establish development standards to implement the Commercial Corridor Comprehensive Plan designation and establish ~~"business districts."~~ the Renton Automall District. These regulations guide the redevelopment of ~~strip commercial urban forms into more concentrated urban forms, provide for design guidelines for residential development within the district, enhance the pedestrian environment, make the commercial environment more attractive, improve the City's tax base, and result in a more successful business district~~ this district. (Ord. 5191, 12-12-2005)

B. APPLICABILITY:

1. Renton Automall District.

a. Automall Area A: That area bounded by Grady Way S. on the north, Rainier Avenue S. (SR-167) on the east, I-405 on the south, and Seneca Avenue S. on the west; and

That area bounded by S.W. Grady Way on the north, Raymond Avenue S.W. on the west, Seneca Avenue S.W. on the east, and the alley midway between S.W. Grady and S.W. 12th Street, on the south.

b. Automall Area B: That area along the south side of S.W. Grady defined by the alley between S.W. Grady Way and S.W. 12th Street on the north, Seneca Avenue S.W. on the east, Raymond Avenue S.W. on the west, and I-405 on the south;

That area along the south side of S.W. Grady Way west of Raymond Avenue S. between S.W. Grady Way on the north, Raymond Avenue S. on the east, a north/south line approximately four hundred feet (400') west of Raymond Avenue S.W. on the west, and I-405 on the south;

That area along the north side of S.W. Grady Way west of Lind Avenue S. bounded by S.W. Grady Way on the south, Oakesdale Avenue S.W. on the west, S.W. 10th Street on the north, and Lind Avenue S.W. on the east;

That area along the north side of S.W. Grady Way between Lind Avenue to the west and Rainier Avenue S. on the east. Beginning at a point approximately four hundred feet (400') north of S.W. Grady Way along the east side of Lind Avenue S.W. on the west, then east for a distance of approximately three hundred twenty five feet (325'), then south to a point approximately one hundred eighty feet (180') north of S.W. Grady Way, then east from this point parallel to S.W. Grady Way to a point approximately ninety feet (90') west of Rainier Avenue S., then north from this point approximately sixty feet (60'), then west approximately fifty feet (50'), and then north approximately two hundred fifteen feet (215') and then east approximately one hundred sixty feet (160') to Rainier Avenue S. on the east;

That area north of South 7th Street and west of Hardie Avenue generally described as the area beginning at the northwest corner of South 7th Street and Hardie Avenue S. and then proceeding west approximately four hundred twenty five feet (425'), then north approximately four hundred fifty feet (450') to the southern edge of the Burlington Northern Railroad right-of-way, then east along the railroad right-of-way approximately two hundred thirty five feet (235') to Hardie Avenue and then south along Hardie Avenue to the beginning point;

That area north of South 7th Street between Hardie Avenue on the west, the Burlington Northern Railroad right-of-way on the north, and Rainier Avenue on the east;

That area north of South 7th Street between Rainier Avenue S. on the west, a line approximately one hundred ninety feet (190') north of and parallel to South 7th Street on the north, and Shattuck Avenue S. on the east;

The triangular area on the south side of South 7th Street between Hardie Avenue on the west and Rainier Avenue on the east;

The larger area north of S. Grady Way between Rainier Avenue on the west and Shattuck Avenue S. on the east between South 7th Street on the north and S. Grady Way on the south;

That area north of S. Grady Way between Shattuck Avenue S. on the west, the northern edge of the former railroad right-of-way approximately one hundred fifty feet (150') north of S. Grady Way, and Talbot Road/Smithers Avenue S. on the east;

That area along the south side of S. Grady Way between SR-167/Rainier Avenue S. on the west and a north/south line approximately one thousand six hundred thirty feet (1,630') east of SR-167 on the east, S. Grady Way on the north, and on the south, west along S. Renton Village Place approximately one hundred seventy five feet (175') to the 1998 zoning boundary between the CA Zone and the CO Zone on the south; and

That area along the south side of S. Grady Way east of Talbot Road bounded by Talbot Road on the west, S. Grady Way on the northwest, Renton City Hall on the north/northeast, Benson Road S. on the east/southeast, and the I-405 right-of-way on the south.

~~2. N.E. Sunset Boulevard Business District: That area (RMC 4-3-040H) along NE Sunset Blvd. from east of Duvall Ave. NE to west of Union Ave. NE.~~

~~3. Northeast Fourth Business District: That area (RMC 4-3-040I) along NE 3rd and 4th Streets between Queen Ave. NE on the west and Field Ave. NE on the east.~~

~~4. Rainier Ave. Business District: The area (RMC 4-3-040J) north of South 2nd Street on the north and the Houser railroad trestle on the south to the Renton Automall District. (Amd. Ord. 4839, 5-8-2000; Ord. 5100, 11-1-2004; Ord. 5331, 12-10-2007)~~

~~5. Puget Drive Business District: The area (RMC 4-3-040K) along Benson Road South and Puget Drive South that is south of Interstate 405 and north of the intersection of Puget Drive South and Benson Road South. (Ord. 5191, 12-12-2005; Ord. 5355, 2-25-2008)~~

C. USES PERMITTED IN THE RENTON AUTOMALL IMPROVEMENT DISTRICT:

The following use provisions take precedence over the underlying zoning:

USES ALLOWED IN AREA A Only the following uses are permitted within Automall Area A	USES ALLOWED IN AREA B
Within the CA Zone: Auto, motorcycle, snowmobile, lawn and garden equipment, and passenger truck sales; Secondary uses including: Licensing bureaus, car rentals, public parking, and other uses determined by the Zoning Administrator to directly support dealerships;	All uses permitted by the underlying zoning

<p>Within the IM Zone: Auto, motorcycle, snowmobile, lawn and garden equipment, passenger truck sales, and existing office; Secondary uses including: Licensing bureaus, car rentals, public parking, off-site parking consistent with RMC 4-4-080E2 and other uses determined by the Zoning Administrator to directly support dealerships.</p>	
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D. DEVELOPMENT STANDARDS FOR USES LOCATED WITHIN THE RENTON AUTOMALL – AREAS A AND B:

All permitted uses in Area A and all auto sales and related uses in Area B of the Renton Automall shall comply with the following development standards:

	ALL USES IN AREA A, DEALERSHIPS AND RELATED USES IN AREA B	NON-DEALERSHIPS AND RELATED USES IN AREA B
SERVICE AREA ORIENTATION	Service areas shall not face public street frontage.	Service areas shall not face public street frontage.
LANDSCAPING – STREET FRONTAGE LANDSCAPING REQUIREMENTS for lots which abut Lind Avenue S.W., S.W. Grady Way, Talbot Road S. (SR-515) and Rainier Avenue S.	A 15-foot-wide landscape strip along these street frontages. This frontage requirement is in lieu of the frontage requirement listed for the zone in chapter 4-2 RMC. Unimproved portions of the right-of-way may be used in combination with abutting private property to meet the required 15-foot landscape strip width. The landscaping shall include a minimum 30-inch-high berm and red maples (<i>Acer rubrum</i>) planted 25 feet on center.	Pursuant to landscaping requirements listed in chapter 4-2 RMC (requirements for the underlying zone) and RMC 4-4-070 .
LANDSCAPING – MINIMUM AMOUNT AND LOCATION	Minimum 2.5% of the gross site area shall be provided as on-site landscaping. Landscaping shall be consolidated and located at site entries, building fronts, or other visually prominent locations as approved through the site plan development review process.	Pursuant to landscaping requirements listed in chapter 4-2 RMC (requirements for the underlying zone) and RMC 4-4-070 .
WHEEL STOPS	If frontage landscaping is relocated, then permanent wheel stops or continuous curbs must be installed a minimum of 2.5 feet from sidewalks to prevent bumper overhang of sidewalks. Where these requirements differ from the requirements of the parking, loading and driveway regulations of chapter 4-4 RMC, these requirements shall govern.	If frontage landscaping is relocated, then permanent wheel stops or continuous curbs must be installed a minimum of 2.5 feet from sidewalks to prevent bumper overhang of sidewalks. Where these requirements differ from the requirements of the parking, loading and driveway regulations of chapter 4-4 RMC, these requirements shall govern.
CUSTOMER PARKING	Customer parking shall be designated and striped near entry drives and visible from public streets. Where possible, customer parking shall be combined with adjacent dealership customer parking and shared access. Where these requirements differ from the	Customer parking shall be designated and striped near entry drives and visible from public streets. Where possible, customer parking shall be combined with adjacent dealership customer parking and shared access. Where these requirements differ from the

	requirements of the parking, loading and driveway regulations of chapter 4-4 RMC, these requirements shall govern.	requirements of the parking, loading and driveway regulations of chapter 4-4 RMC, these requirements shall govern.
AUTOMALL RIGHT-OF-WAY IMPROVEMENT PLAN COORDINATION	Once completed, all development shall coordinate with a right-of-way improvement plan. A right-of-way improvement plan shall be completed by the City in coordination with adjacent property owners, and shall address gateways, signage, landscaping, and shared access.	Once completed, all development shall coordinate with a right-of-way improvement plan. A right-of-way improvement plan shall be completed by the City in coordination with adjacent property owners, and shall address gateways, signage, landscaping, and shared access.
AUTOMALL IMPROVEMENT PLAN COMPLIANCE	All development shall coordinate with the Automall Improvement Plan adopted by Resolution No. 3457. The plan addresses potential street vacations, right-of-way improvements, area gateways, signage, landscaping, circulation, and shared access.	All development shall coordinate with the Automall Improvement Plan adopted by Resolution No. 3457. The plan addresses potential street vacations, right-of-way improvements, area gateways, signage, landscaping, circulation, and shared access.
MODIFICATIONS		Where full compliance with these provisions would create a hardship for existing uses undergoing major modifications, the Zoning Administrator may modify them. Hardship for existing uses may result from existing lot coverage, existing siting of buildings, etc., which preclude full compliance.

(Amd. Ord. 5355, 2-25-2008)

E. POTENTIAL WAIVER OF STREET VACATION FEES FOR DEALERSHIPS LOCATED WITHIN THE RENTON AUTOMALL AREA A:

All street vacation fees and compensation for the right-of-way may be waived by the Council for developing properties in Area A, provided:

1. The properties are designated to be vacated on the Automall Improvement Plan Map,
2. The application for street vacation conforms to RMC [9-14-10](#), Administrative Procedure for Right-of-Way Vacations, and
3. The uses proposed conform to subsection C of this Section. (Amd. Ord. 4749, 10-19-1998)

~~F. DEVELOPMENT STANDARDS FOR USES LOCATED WITHIN THE NORTHEAST FOURTH STREET, PUGET DRIVE, RAINIER AVENUE, AND SUNSET BOULEVARD BUSINESS DISTRICTS:~~

~~1. Northeast Fourth, Puget, Rainier, and Sunset Business Districts:~~

~~**a. Maximum Front Yard Setback:** Maximum front setback of fifteen feet (15') from the property line. In the NE Fourth Business District, the fifteen foot (15') setback may be modified to accommodate the Boulevard Improvement Plan. When the fifteen foot (15') setback is modified, a fifteen foot (15') landscaped buffer shall be required within the enlarged setback. Required parking shall not be located within a modified setback.~~

b. Public Plazas: There shall be provision of a public plaza of no less than one thousand (1,000) square feet with a minimum dimension of twenty feet (20') on one side abutting the sidewalk. The pub-

lic plaza must be landscaped consistent with RMC 4-4-070, including at minimum street trees, decorative paving, pedestrian-scaled lighting, and seating. These public plazas are to be provided at all intersections in the business districts at the intersections identified:

- i. In the NE 4th Business District, any intersection with NE 4th Street.
- ii. In the Sunset Business District, any intersection with Sunset Boulevard.
- iii. In the Puget Business District, at the intersection of Benson Road and Puget Drive.
- iv. In the Rainier Avenue Business District, at the intersections of Rainier Avenue and South 3rd, as well as Rainier Avenue and Airport Way.

c. Future Commercial Development Pads: For parcels that are not fully developed, designate appropriate areas for future pad development to occur in later phases.

d. Parking: The maximum number of parking spaces provided for uses within the corridor designation is limited to the minimum requirement in RMC 4-4-080F10, Number of Required Parking Spaces. Garage structures shall not open directly onto a principal arterial or street. Parking lots shall be oriented to minimize their visual impact on the site. No more than six (6) stalls may be consecutively clustered without an intervening landscaped area a minimum of five feet (5') in width and the length of the stall.

e. Pedestrian Connections:

i. Location of Pedestrian Connections:

- (a) A minimum of one pedestrian connection shall be provided to connect the entry or entries of each detached building to the street in addition to sidewalks required in RMC 4-6-060F.
- (b) A minimum of one pedestrian connection shall be provided from each parking field located on the back and/or side of a building to the entry or entries.
- (c) A minimum of one pedestrian connection shall be provided from each side of a property or development abutting or adjacent to commercial and/or residential uses.
- (d) Space for the minimum required pedestrian connections above shall be reserved for future pad development and when the proposed development is abutting or adjacent to an unused parcel.

ii. Design Standard for Internal Pedestrian Connections:

- (a) Pedestrian connections shall be ADA-accessible and a minimum of five feet (5') in width.
- (b) At least one of the following materials shall be used to define the walkway: pavers, changes in texture, or changes in the composition of the paving.
- (c) The entry and exit of the walkway shall be defined with a trellis, special railing, bollards, or other architectural features, as approved by the Reviewing Official.
- (d) Planting strips required in RMC 4-6-060F shall be located between the road and the required sidewalk. Trees, shrubs, ground covers, and perennial planting are required components of landscaping. Landscaping is subject to the requirements of RMC 4-4-070.
- (e) Bollards or other decorative features may be provided at the pedestrian access points between commercial and residential uses. Chains across vehicular or pedestrian access points are prohibited.

f. Additional Standards for Stand Alone Residential Uses: Site design shall include design elements that support a quality mixed-use business district. The following minimum standards shall be met; however, the Reviewing Official may require additional elements consistent with site plan review criteria when determined necessary to integrate commercial and residential uses within this district.

i. Street Grid: The project shall use a modified street grid system where residential buildings are oriented to a street. A public street grid system within the project shall be provided. No cul-de-sacs allowed. Hammerhead turnarounds may only be used if the ends are able to accommodate future connection as part of the modified street grid system. Emergency fire access shall be provided through public streets or private easements connecting to the adjacent commercial or residential area.

ii. Site Design: Each unit shall address the public street, private street, or court with a private residential entry on the front facade of the structure designed to provide individual ground floor connection to the outside.

iii. Residential Building Size: A maximum of four (4) consecutively attached units shall be allowed.

iv. Minimum Land Area: A minimum of one thousand two hundred (1,200) square feet of land area per dwelling unit is required. Each dwelling shall have a ground-related private useable outdoor space of at least two hundred (200) square feet with a minimum dimension of ten feet (10').

v. Building Design Standards: Urban Design Regulations District B standards shall be required. See RMC 4-3-100. Distinctive building design shall be provided with a superior level of quality for materials, details, and window placement. A consistent visual identity shall be applied to all sides of building that can be seen by the general public. Buildings should integrate pitched roofs, dormer windows, etc., to illustrate residential massing. Variation of modulation of vertical and horizontal facades of a minimum of six feet (6') depth and twenty feet (20') length is required at a minimum of a forty-foot (40') interval to reduce overall bulk and add interest and quality. Facades may be articulated with bays, terraces, balconies, awnings, stoops, recessed openings, etc. Large "boxes" without articulation are not allowed. No parapet or roof line shall exceed one-half the length of the building facade without a change in elevation. Building entries should be the most prominent feature of the facade, emphasized through the use of materials and architectural detail such as towers, projections, varied roofs, trellis work, pergolas, or covered entryways.

vi. Walling and Fencing: Any walling or fencing shall use materials used in the architectural treatment of the dwellings. In addition, where fencing occurs between residential and commercial uses, a minimum of one pedestrian access point shall be required consistent with the standards above.

vii. Additional Residential Parking Standards: Parking must be within an enclosed structure located to the rear of the primary structure or in a detached garage with rear access. If this absolutely cannot be accomplished due to physical constraints of the site, then garages shall be designed to have minimum impact on streetscape appearance and function through the use of shared drives, architectural detailing, or facade design. The required guest spaces for attached residential uses may be surface parking.

g. Additional Standards for Mixed Use (Within the Same Building) Commercial and Residential Uses: Site design shall include design elements that support a quality mixed use business district. The following minimum standards shall be met; however, the Reviewing Official may require additional elements consistent with site plan review criteria when determined necessary to integrate commercial and residential uses within this district.

~~i. **Access:** Hammerhead turnarounds may only be used if the ends are able to accommodate future connection as part of a modified street grid system. Emergency fire access shall be provided through public streets or private easements connecting to the adjacent commercial or residential area.~~

~~ii. **Site Design:** Commercial space must be reserved on the ground floor of all mixed use buildings, at a minimum depth of thirty feet (30') along the street frontage on the ground floor in the NE 4th, Sunset, Rainier, and Puget Business District Overlay. Residential uses shall not be located in the ground floor commercial space, except for a residential entry feature linking the residential portion of the development to the street.~~

~~iii. **Building Design Standards:** Urban Design Regulations District B standards shall be required. See RMC 4-3-100. Distinctive building design shall be provided with a superior level of quality for materials, details, and window placement. A consistent visual identity shall be applied to all sides of building that can be seen by the general public. Variation of modulation of vertical and horizontal facades of a minimum of six feet (6') depth and twenty feet (20') length is required at a minimum of a forty-foot (40') interval to reduce overall bulk and add interest and quality. Facades may be articulated with bays, terraces, balconies, awnings, stoops, recessed openings, etc. Large "boxes" without articulation are not allowed. No parapet or roof line shall exceed one-half the length of the building facade without a change in elevation. Building entries should be the most prominent feature of the facade, emphasized through the use of materials and architectural detail such as towers, projections, varied roofs, trellis work, pergolas, or covered entryways.~~

~~iv. **Additional Mixed Use Parking Standards:** Parking for the residential units must be within an enclosed structure located under the residential portion of the building. The required guest spaces for residential uses may be surface parking.~~

~~2. Rainier Avenue Business District: The district shall have the following additional requirements:~~

~~a. Access points are to be consolidated to properties.~~

~~b. New billboards prohibited.~~

~~c. Freestanding signs are restricted to monument signs.~~

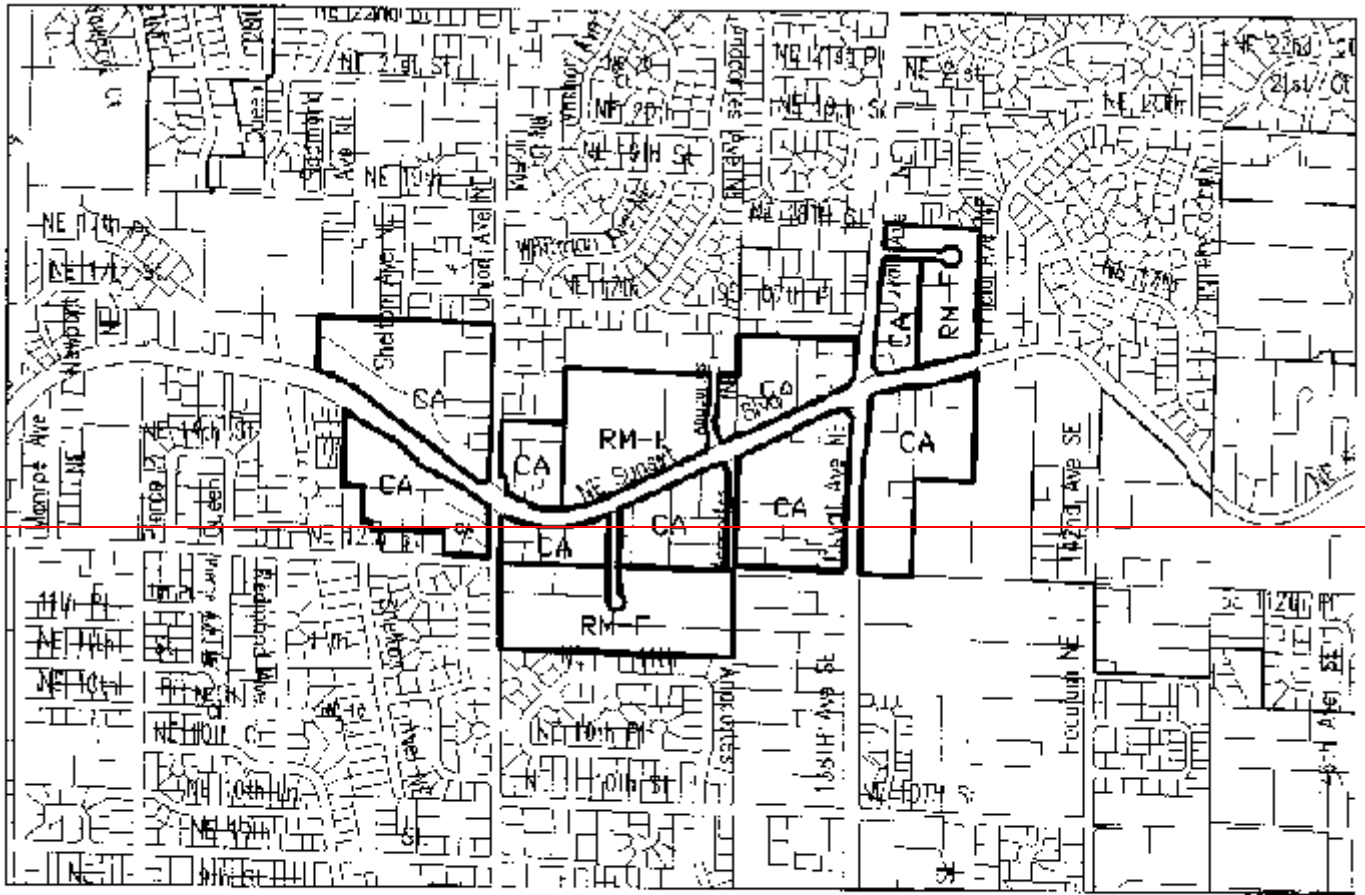
~~d. Sidewalk width at the intersections of Rainier Avenue and SW Sunset Boulevard/South Third Street, Rainier Avenue and South Third Place, and Rainier Avenue and South Fourth Place shall be ten feet (10'), minimum.~~

~~e. Maximum setback of fifteen feet (15'). Building setback for a primary use may exceed the maximum; provided, that a designated area for a future pad development that will conform to the maximum setback is established through a recorded document.~~

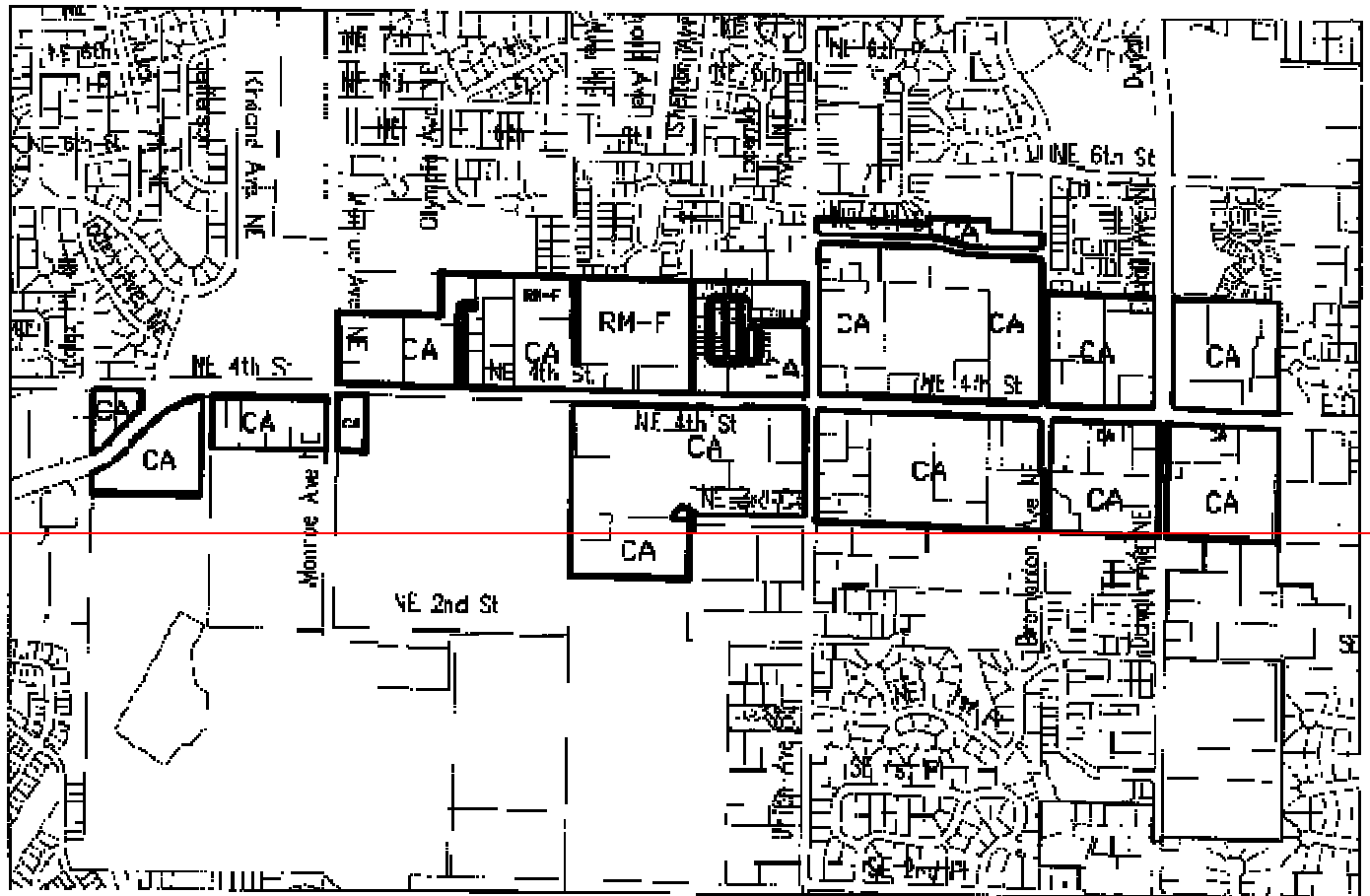
~~f. The number of parking spaces provided for uses within the district is limited to the minimum requirement. (Amd. Ord. 5331, 12-10-2007)~~

FG. MAPS OF AUTOMALL OVERLAY DISTRICTS:

H. NE SUNSET BOULEVARD CORRIDOR BUSINESS DISTRICT:



I. NE 4TH STREET CORRIDOR BUSINESS DISTRICT:

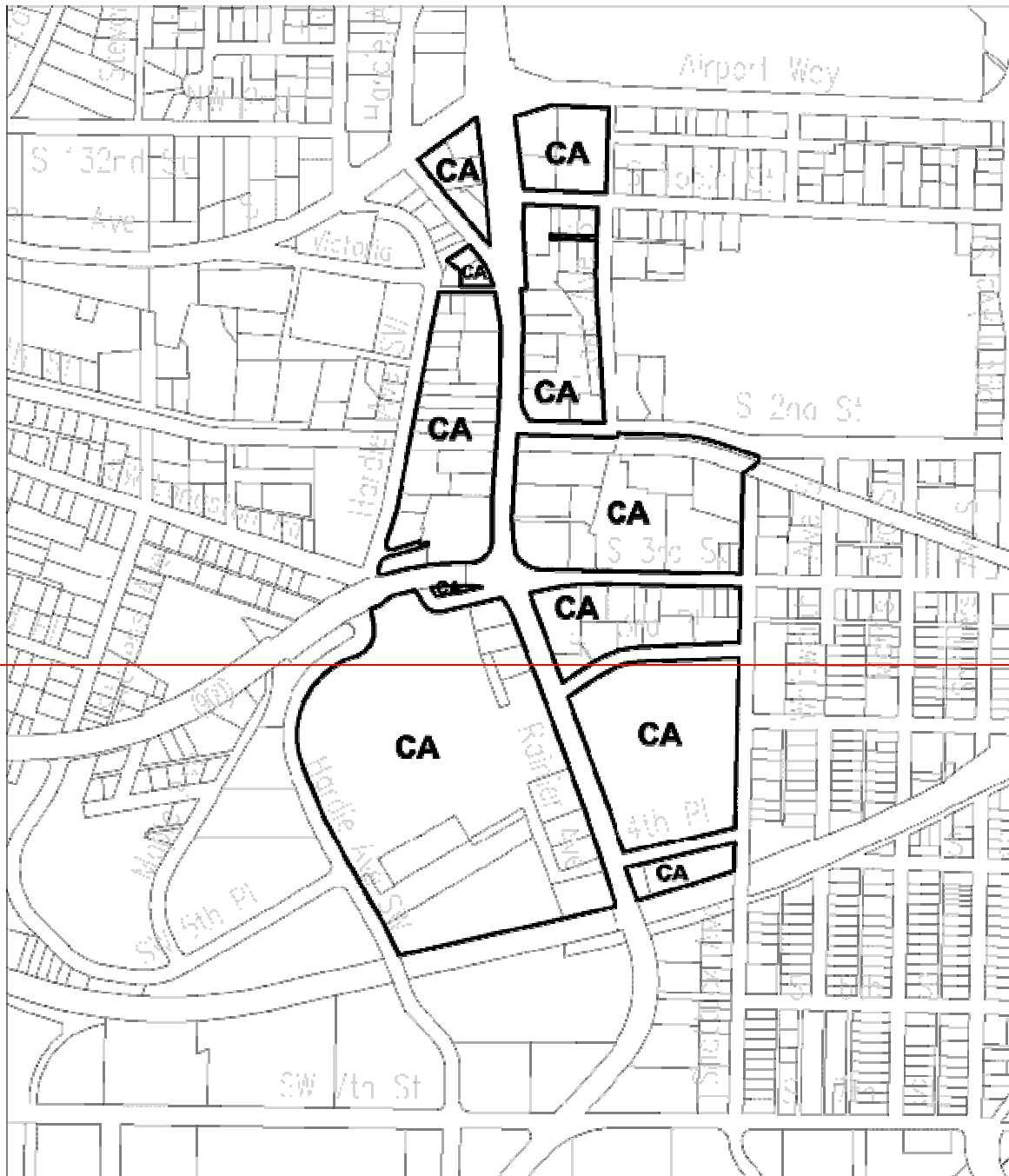


NE 4th Street Business District



(Ord. 5191, 12-12-2005)

J. RAINIER CORRIDOR BUSINESS DISTRICT:



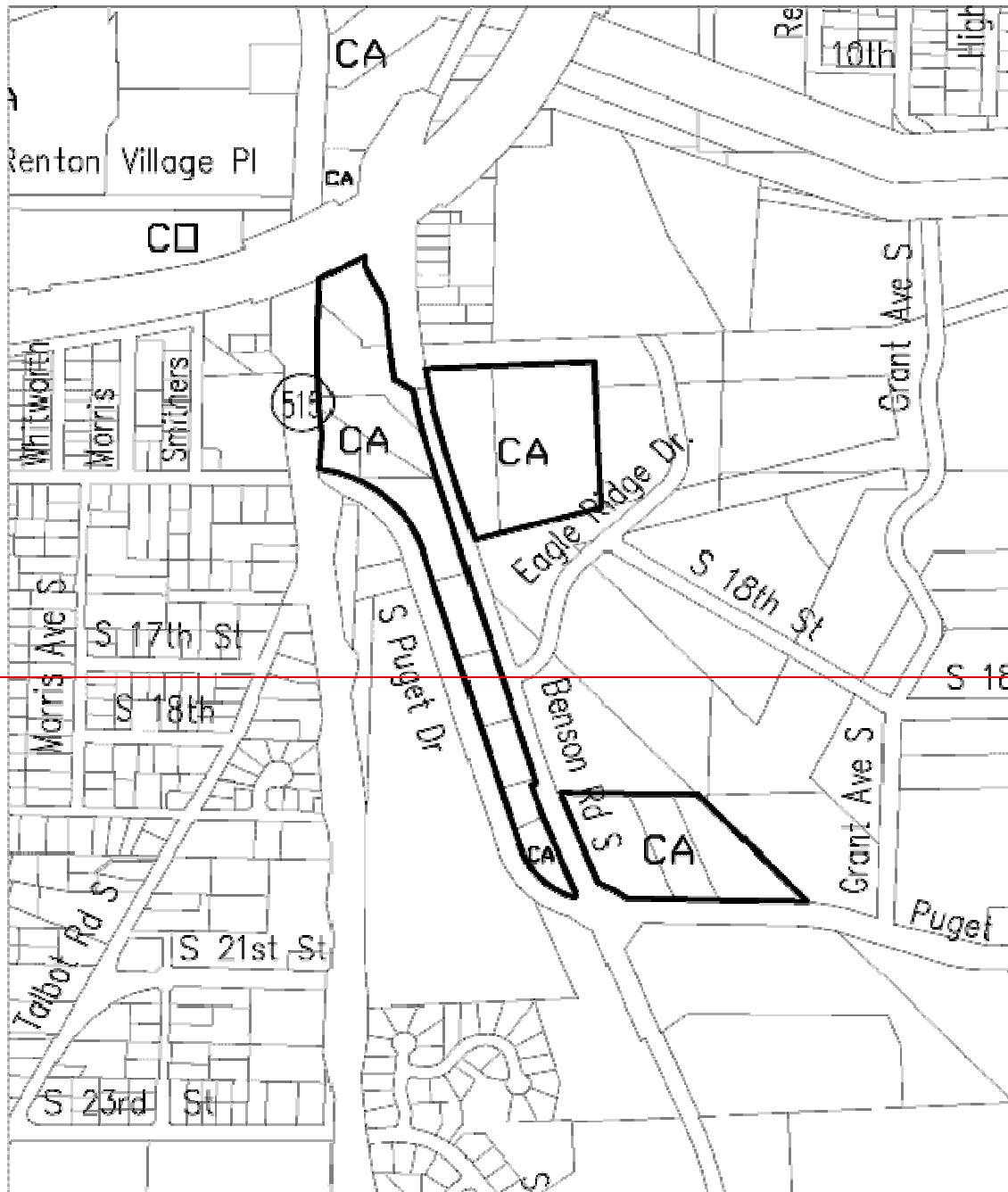
Rainier Business District



Economic Development, Neighborhoods & Strategic Planning
Alex Pletsch, Administrator
C.F. Fernald
14 June 2007

(Ord. 4839, 5-8-2000; Ord. 5100, 11-1-04; Ord. 5191, 12-12-2005; Ord. 5331, 12-10-2007)

K. PUGET DRIVE BUSINESS DISTRICT:



Puget Drive Business District



Strategic Development, Neighborhoods & Strategic Planning
City of Renton
311 City Center Blvd
Renton, WA 98057



(Ord. 5191, 12-12-2005)